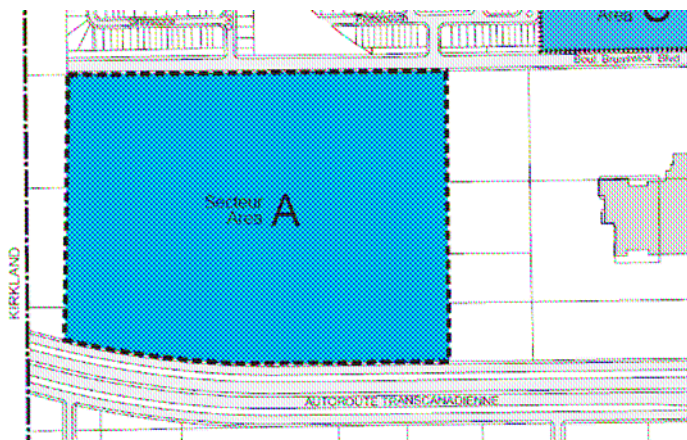


Chapter 3 PROVISIONS APPLICABLE TO SECTOR "A"

Area subject to the provisions of the present chapter

- 26.** The provisions of the present chapter shall apply to the area identified as Sector "A" on the "Plan of the areas subject to the by-law", appended hereto as Appendix "1" to form an integral part hereof.

Operations subject to the prior approval of a site planning and architectural integration program



- 27.** Within Sector « A », the delivery of a subdivision permit and the delivery of a building permit or certificate of authorization to allow the construction of, an addition to or an alteration of a building or the landscaping of a landsite, whether such landscaping includes a parking area or not, are subject to prior approval, by Council, of a site planning and architectural integration program according to the procedure set in Chapter 2 of the present by-law.

Moreover, in this sector, no tree shall be felled before a site planning and architectural program has been approved for the landsite on which such tree is located, except if the tree has to be felled for one of the reasons mentioned in paragraph 4 of Article 20 of the present by-law.

Objectives

- 28.** Within Sector "A", objectives according to which an application for the approval of a site planning and architectural integration program shall be reviewed are as follows:
1. Ensure the development of a truly urban complex, lively, suitable for exchanges and apt to become the city centre of the City of Pointe-Claire and, possibly, of the whole of the West Island.
 2. In the part of Sector "A" adjacent to Brunswick Boulevard, ensure the development of a quality apartment complex – or a complex combining apartments, commerces and offices – integrated with a downtown type urban ensemble and adapted to the density and the diversity of the traffic on the inner and peripheral street network.
 3. In the part of Sector "A" adjacent to the Trans Canada Highway, ensure the development of a complex combining offices and retail commerces of great architectural quality, arranged around a public area where pedestrian circulation will be prioritized.

4. Promote design excellence and originality while ensuring a certain architectural homogeneity in the overall project.
5. Protect and enhance the best components of the existing woods.
6. Minimize the impacts of the project on the environment, on neighbouring residential areas and on traffic.
7. Encourage non-motorized trips.
8. Limit the impacts of high-rise buildings on wind and sun lighting.

Criteria

- 29.** Within Sector "A", conformity of a site planning and architectural integration program to the objectives set in Article 28 shall be assessed according to the following criteria.

Criteria pertaining to subdivision

- 30.** With regard to subdivision, the criteria according to which the site planning and architectural integration program will be assessed are the following:
 1. In the case where Council would require as a preliminary condition prescribed in accordance with section 117.1 of the Act respecting land use, planning and development (R.S.Q. c.A-19-1) that the owner commit to cede without cost to the City a parcel of land which, in Council's opinion, is suitable in the establishment or enlargement of a park or playground or in the preservation of a natural area, or in the case where Council would demand that the owner make such a commitment and remit an amount to the municipality, such area to be transferred for the establishment or enlargement of a park or playground or for the preservation of a natural area, in accordance with the provisions of the Subdivision by-law, should be located, in order of preference, first in area 367-02, then in area 367-04, then in area 367-03 on the map of the forest survey of Appendix "2" of the present by-law.
 2. In order to minimize the impacts on the traffic on the public thoroughfares, the location of vehicular accesses to each of the lots should take into consideration, among other things, the location of the vehicular accesses to the other lots served by these thoroughfares.
 3. The delimitation of the lots, the location of the buildings and the planning of the parking areas and circulation aisles should aim at saving as many mature trees as possible.

Criteria pertaining to siting

- 31.** With regard to siting, the criteria according to which the site planning and architectural integration program will be assessed are the following:
 1. The natural topography of a landsite should not have to be substantially modified, except if the landsite is lower than the street and lower than the

adjacent properties, in which case it could be filled, but not to a point where its final grade would exceed the level of the adjacent properties.

2. The way the building is sited and its surrounding developments (recreational spaces, parking areas and circulation aisles, etc) are designed should demonstrate the intent of the designer to preserve and enhance as many mature trees as possible, other than poplars of all species.
3. The buildings should be sited in an organized manner according to an obvious geometric and functional arrangement.
4. No building should be located at less than 25.0 metres from the right-of-way of the Jacques-Bizard corridor.
5. The minimum setback of any building from any public thoroughfare other than the Jacques-Bizard corridor should be equal to half of its height, without being less than the minimum front setback as established in the Zoning by-law.
6. As much as possible, the buildings should be designed and sited in such a way that the garage entrances and the service areas will not be visible from the public thoroughfare.

Criteria pertaining to architecture

32. With regard to architecture, the criteria according to which the site planning and architectural integration program will be assessed are the following:

1. Any construction should be of high quality. The materials or combinations of materials should demonstrate a concern for authenticity and restraint. The choice of color of a given material could be refused if such color is not compatible with the character of the neighbourhood. The facing materials should be precast concrete panels, brick or stone (natural or artificial) masonry, glass or any other material considered as equivalent in quality by the Planning Advisory Committee and Council.
2. The architectural treatment of a building should be in keeping with the neighbouring buildings.
3. The architectural treatment of entirely commercial or office buildings should differ from that of the residential buildings while showing a will of integration.
4. All the facades of a building should have the same unified and coherent treatment; the same material or combination of materials should be used on all the facades of the building, whether these facades are visible from the street or not.
5. Any non-rooftop mechanical equipment that could be visible from a public thoroughfare or public area should be integrated into the building or hidden by a screen that is integrated into the architecture of the building.

6. At least half of the parking spaces required according to the Zoning by-law for office uses should be located inside a building and below the level of the ground.
7. No building of more than 6 storeys or 23 metres in height should be designed or sited in such a way as to hinder the sun lighting of an habitable room in a residential building at noon on June 21 or in such a way as to unduly shade a neighbouring property.
8. No building of more than 6 storeys or 23 metres in height should be designed or sited in such a way as to:
 - i) generate a wind impact whose average speed at ground level, calculated on an hourly basis, would be greater than 15 km/h in the winter and/or 22 km/h in the summer more than 25% of the time in a public thoroughfare and/or more than 10% of the time in a park, a public square or a rest area.;
 - ii) generate gusts of wind at ground level whose speed exceeds 75 km/h at ground level for more than 1% of the time, the reference period for evaluating gusts of wind being 2 seconds or less, with a turbulence of 30%.
9. Any addition or alteration to an existing building should be in keeping with its original character and should integrate to its architecture and its style.

Criteria pertaining to the development of the site

- 33.** With regard to the development of the site, the criteria according to which the site planning and architectural integration program will be assessed are the following:
1. As many as possible of the mature trees that constitute the woods of better quality according to the forest survey of Appendix "2" of the present by-law should be preserved.
 2. Pedestrian and cycle paths' networks should be designed in order to facilitate safe circulation within the property and easy links with the existing or planned networks in the neighbouring sectors.
 3. Any possibility of outdoor storage should be excluded.
 4. Garbage storage areas should be integrated into the architecture of the buildings and should not be visible from the public thoroughfares or from pedestrian pathways; they should be designed to minimize impacts, namely noises and odours.
 5. Loading and unloading docks should be located and developed as to minimize the impacts associated with vehicular traffic and with delivery and shipping operations, especially near residential buildings.

Additional criteria applicable to the residential or mixed use area

- 34.** Within the north half of sector "A", conformity of a site planning and architectural integration program to the objectives set in Article 28 shall also be assessed according to the following criteria:
1. Residential buildings should be quality built, with large rooms, extensive fenestration, balconies and good sound-proofing; they should be designed and sited in order to ensure adequate sun lighting of the apartments.
 2. With regard to architectural design and landscaping, measures should be taken to ensure the privacy of the residents and protect them from noise and other inconveniences associated with commercial activities and traffic; apartments or dwellings at ground level or on the second floor, with windows or exterior spaces (decks or balconies) at less than 30.0 metres from the Jacques-Bizard corridor or Brunswick Boulevard should, as much as possible, be avoided.
 3. Commercial and/or office activities should be separated from the residential uses: they should be located in separate buildings or on the lower floors of the residential buildings.
 4. In the case where a building houses both commercial or office uses and residential uses, entrances to commercial or office uses should be distinct from the entrances to the residential uses and the treatment of the part of the building housing commercial or office uses should be different from the residential part, while using the same materials or combinations of materials.
 5. Any residential or partially residential complex should have at least two recreational facilities such as a pool, tennis courts or a fitness room.
 6. No more than 50% of the lot area should be occupied by impervious surfaces; for the purposes of this provision, a "green roof" shall be considered as pervious.
 7. All the area located within 15 metres from Brunswick Boulevard and, if not sufficiently wooded, all the area located within 25 metres from the right-of-way of the Jacques-Bizard corridor, should be planted with trees and shrubs in sufficient number and size to constitute a buffer zone dense enough to protect the privacy of the residents.
 8. There should be no building, construction or other accessory structure detached from the principal building other than a pool shed; the latter should only house the mechanical equipment required for the operation of the pool, changing rooms, showers and an area to store garden furniture off-season.
 9. All the parking spaces required according to the Zoning by-law for the residential uses should be indoor and below the level of the ground, except the spaces intended for visitors which shall be necessarily located outside and close to the main entrances to the buildings.

Additional criteria applicable to the commercial area

35. Within the south half of Sector "A" adjacent to the Trans-Canada Highway, conformity of a site planning and architectural integration program to the objectives set in Article 28 shall also be assessed according to the following criteria:

1. The buildings should be sited around a central pedestrian area.
2. The main facade of the commercial buildings should open onto that area and pedestrians should mainly access to the buildings from that area.
3. The main facade of the buildings should reflect their function (retail commercial or offices) and contain as many openings as possible on the central area in order to contribute to its liveliness.
4. The entrances to the commercial buildings should be clearly visible from the public area.
5. Without necessarily being identical, the buildings should display an integrated architectural treatment using a same architectural vocabulary in its shapes, lines, scale, colours and decorative elements.
6. Bright and intense colours should be used in moderation and reserved to highlight certain building details.
7. Parking areas should preferably be located underground or, if not possible, on the periphery of the complex.
8. Any outdoor parking area should be separated from any lot occupied by a residential building and from any public thoroughfare by a green strip at least 6 metres in width and planted with trees and shrubs.
9. To avoid large expanses of asphalt and provide a few shaded spaces, all outdoor parking area should be broken into parcels no more than 6,000 square metres in area and separated by grassed islands planted with trees.
10. Any parking lot more than 1,000 square metres in area should be equipped with sidewalks or paths to provide safe pedestrian circulation.
11. All parking structures should harmonize with the commercial buildings and be of an at least equivalent architectural quality.
12. No office building or other commercial building should be located at less than 25.0 metres from any lot occupied (or to be occupied) by a residential building.